



***OFF-STREET  
RESIDENTIAL PARKING  
IN CONSERVATION AREAS***

PLANNING AND DEVELOPMENT SERVICES

BRISTOL LOCAL PLAN  
POLICY ADVICE NOTE 6



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## 1. INTRODUCTION

This advice note is one of a series of documents which aim to encourage high standards of development and environmental quality in Bristol. It provides supplementary planning guidance for householders, designers and their clients on how to achieve the design principles contained in Policy B17 of the Bristol Local Plan.

**Policy B17:** *The City Council will seek to retain and enhance traditional townscape features that contribute to the character of Conservation Areas.*

*In particular the City Council will resist:-*

- (i) The erosion of traditional landscapes, open spaces and gardens.*
- (ii) The introduction of car parking in areas traditionally used as gardens and forecourts where it erodes either the character of the street and/or the setting of historic buildings.*
- (iii) The loss of traditional paving materials, walls, railings, piers and gate posts, signs, nameplates, lamp posts, seats and other ironmongery, while the additions should respect the particular traditions of the locality or be discreetly located so as to minimise intrusion into the local area.*

Bristol City Council is committed to promoting good design. Altering your home to a high standard can add value to your property and enhance the character of your local area. The City Council will take a positive view of development proposals which follow the advice set out below.

You should discuss any proposals to create a hardstanding with the City Council by first writing to the Planning and Development Services Directorate enclosing sketches to illustrate your proposals. The Council can then clarify whether consent is required.



The introduction of car parking has destroyed the character of many areas

This design guide identifies the principles and illustrates the detailed elements that should be considered when contemplating parking provision in either the front or rear garden area.



The first section of this leaflet looks at parking in front gardens and forecourt areas, identifies the importance of retaining the means of enclosure, characteristic landscape and traditional features, and examines the use of materials, both new and re-used. The second section addresses the rear of properties and the potential for parking in rear gardens and forecourts. It considers the need to maintain the backland character, the retention of traditional features and use of materials.

The small size and width of many garden areas means that a parking space is unacceptable as it would have a damaging effect on the visual quality, character and appearance of the property and locality in general. Where a garden is large enough to accommodate some car parking, it should be designed to minimise intrusion, retain the original traditional means of enclosure and, as a guide, take no more than 50% of either front or rear garden areas (but not both).

In many cases the rear of properties were historically used to accommodate coach houses, accessed through rear alleyways. Where such access exists it is desirable that rear garden areas be used to provide car parking, to maintain the character and appearance of the front garden.

Consideration should also be made to pedestrian safety. The position of a hardstanding and pavement crossover may be unacceptable to the Local Highways Authority as vehicles manoeuvring in and out may have difficulty seeing passing pedestrians.

### **Listed Buildings and Conservation Areas**

Some of Bristol's older houses have been "Listed" and appear on the Secretary of State's "List of Buildings of Special Architectural or Historic Interest". These buildings have been registered to protect their character, and ensure that any alterations to them are suitably well designed. If the property is a listed building the boundary wall and any other structure within the curtilage is also Listed.

Where a proposed alteration will affect the character of a Listed Building, a special consent from the City Council is required. To find out if your property is 'Listed' you should contact the Information Services Section of the Planning Directorate.

Also, a number of Bristol's older areas have been designated as Conservation Areas, the character or appearance of which it is desirable to preserve or enhance. However, their special character is due not only to the architectural merit of the buildings but also to the context and landscape in which they are set. The planning authority will seek to maintain or enhance the character and appearance of these areas.

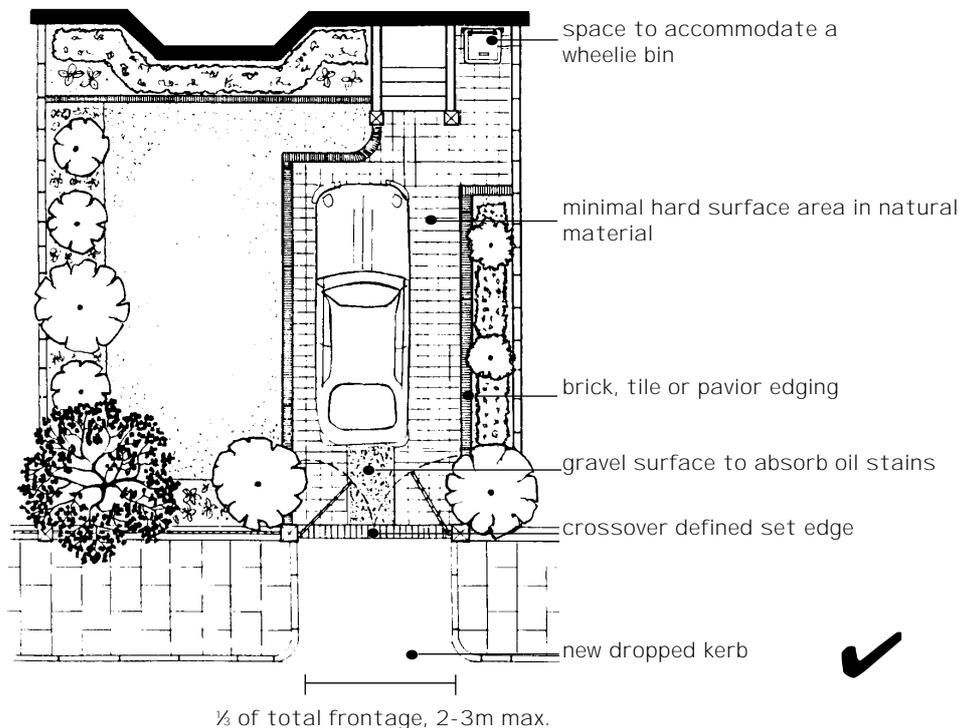
There is a rich diversity in the character and appearance of Bristol's 29 Conservation Areas, each deriving from its historical period of development. Areas such as Clifton, Cotham, Kingsdown and Redland typify this diversity, each having a distinct local identity characterised by their layout, architectural style, use of materials and landscaping.



## 2. PARKING IN FRONT GARDENS AND FORECOURT AREAS

The means of enclosure or boundary wall at the front of a property is a significant feature that needs to be retained. Where an existing entrance is to be widened, original features such as gates and piers should be retained and re-used within the new design. Existing materials should also be reused or replaced to match the existing, or those commonly used in the street.

### General Principles

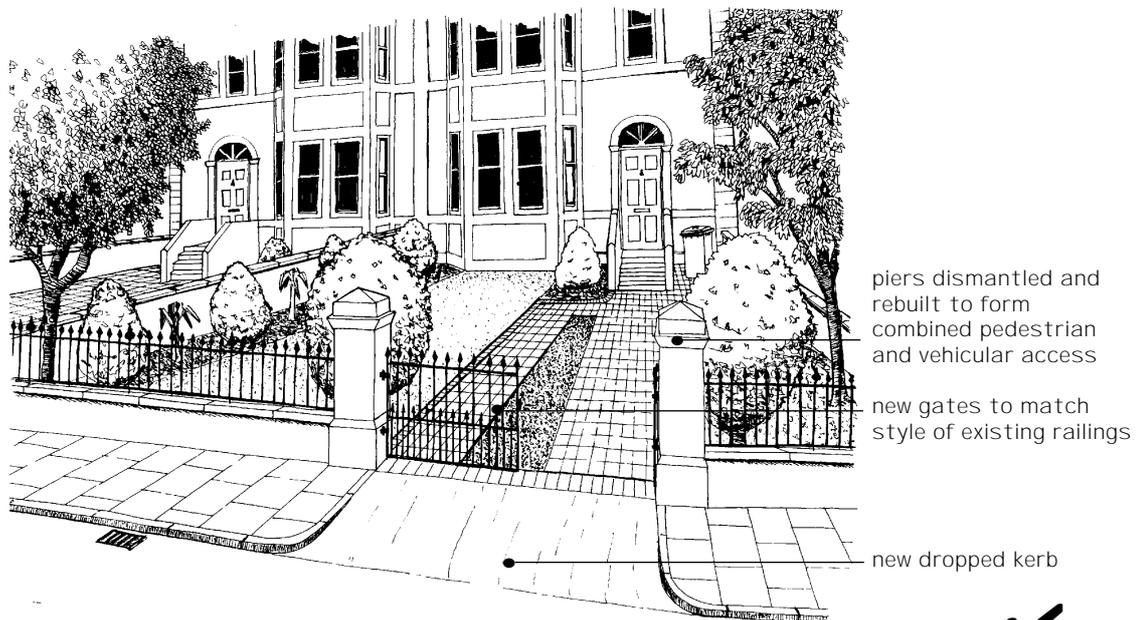


### Details to be incorporated into front garden parking

- \* You should seek to maintain the character of your front garden by reinforcing the soft landscape setting of your house through appropriate planting, screening and surface materials. The garden should be of sufficient depth to provide adequate screening.
- \* A means of enclosure to a property should be retained. Walls, railings and gates should form an integral part of the design. Gates should open inward and not out onto the pavement / highway.
- \* The width of any access should be kept to a minimum. As a guide an opening between 2 - 3 metres should be adequate to accommodate a car. In order to maintain a balance between the boundary wall and any opening it is recommended that the opening takes up no more than a third of the frontage.



- \* A combined entrance should be used where possible for both pedestrian and vehicular access to minimise the visual impact of the opening.
- \* Existing gates and piers should be retained and reused within the new design, with traditional materials used to match the existing. Where walls and railing are missing, reduced in height or otherwise previously altered, they should be reinstated to match the form, details and materials of the original design or street in general.



Front Garden Parking

*Details*

The detailing of any scheme needs careful consideration. It is advised that an agent or architect is used to draw up the proposals. The following elements should be considered:

Walls - the materials, pointing of joints, the coping or wall capping material and detailing, such as quoins;

Piers - the scale, materials, detailing and feature cappings;

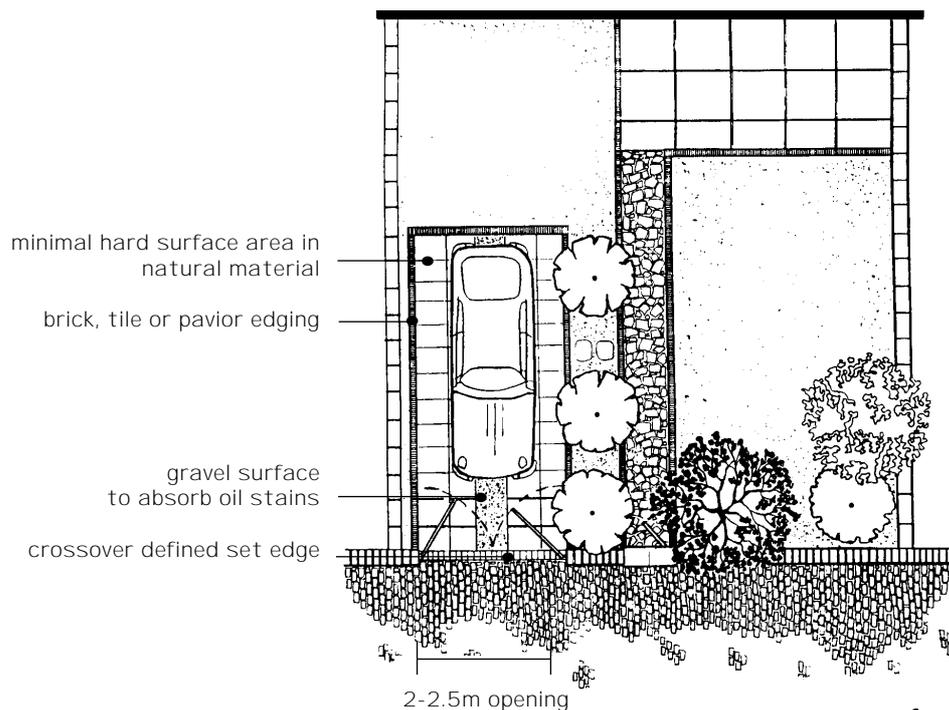
Gates and Railings - the ironwork detail and the style of ornamental finials common to the area;

Surface Materials - the colour, texture and permeability of the material.



### 3. THE REAR OF PROPERTIES

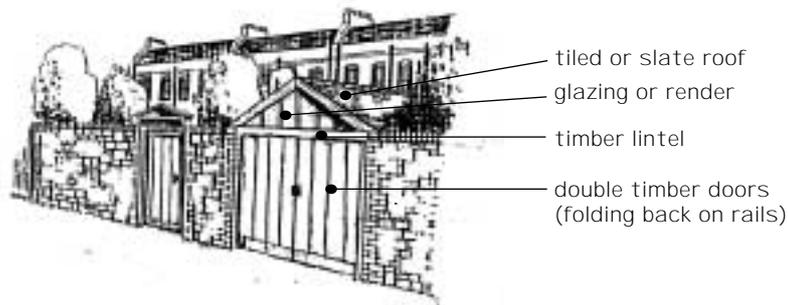
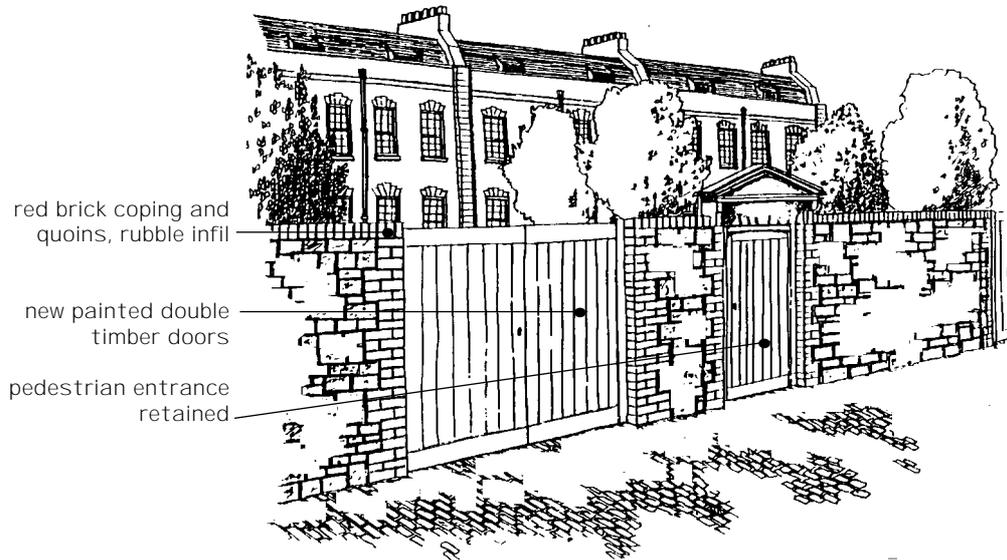
The character at the rear of a property is often very different to that of the front. In many cases, high boundary walls or fences provide a greater level of privacy and security. This character should be retained and so the addition of a hardstanding may require alternative special treatment to that at the front



Details to be incorporated into rear garden parking

#### *General Principles*

- \* A means of enclosure should be retained. The rear boundary wall is traditionally built to a greater height than the front (2 - 2.5 metres), to provide greater security and privacy.
- \* Masonry walls and solid timber doors should form an integral part of the design. A strongly defined rear boundary can either be achieved in this way or through the construction of a garage to echo traditional out-buildings with a separate pedestrian entrance.
- \* Original features, such as pedestrian doorways, quoins or copings, should be retained and existing traditional materials re-used. New materials should match the form and details of the original design or those more commonly used.



New Garages



Rear Garden Parking

*Details*

The detailing of any scheme needs careful consideration. It is advised that an agent or architect is used to draw up the proposals. The following elements should be considered:

Walls - the materials, pointing of joints, the coping or wall capping material and detailing such as quoins;

Piers - the scale, materials, detailing and feature cappings;

Gates - the style, material and finish, traditionally painted timber;

Surface Materials - the colour, texture and permeability of the material.



#### 4. A CHECKLIST OF POINTS

##### *The Hardstanding*

Planning permission is not required for a hardstanding if it is within the curtilage of a single dwelling house. If the property has been subdivided into flats planning permission is required to build a hardstanding. All commercial properties will require planning consent to construct a hardstanding.

##### *Access to Roads*

If you are proposing a new vehicular access to a public road, a pavement cross-over ('haulingway') must be constructed to the satisfaction of the Local Highway Authority. Planning permission is not required for any means of access to a non-trunk or non-classified road, where it is required in connection with other permitted development.

##### *Listed Buildings*

To carry out works to a listed building you will first require Listed Building Consent from the City Council. Any alterations, extensions or demolition of the building, out-buildings or boundary walls or other structure to form the access or create a parking area will require Listed Building Consent.

##### *Conservation Areas*

Conservation Area Consent is required to remove or demolish a boundary wall, gate, fence or railing over one metre in height and abutting a highway if the property is within a Conservation Area.



Car parking which retains the character of a Conservation Area

##### *Trees*

Trees are protected if they are in a

Conservation Area, or are subject to a Tree Preservation Order. In Conservation Areas, all mature trees over 7.5 cm in diameter at a point 1.5 metres above the ground are protected and should not be felled, pruned or cut at the roots without notification to the local planning authority. The owner is required to give at least six weeks notice of their intention prior to the works being carried out. Any size trees with Preservation Orders on them require approval from the local planning authority before works are carried out.



## 5. ADVICE AND FURTHER INFORMATION

This guidance is intended to help you towards an appropriate design solution that retains the traditional and original features of your property. In most cases it is advisable to consider using a competent agent or architect who has experience of this work in order to achieve a good design solution. Planning Officers can give further advice if you make an appointment with them.

A Planning Officer can be contacted through our Reception staff, preferably by writing or telephoning to make an appointment. An appointment will ensure that Planning Officers can make time to see you and maximize the benefit of your visit.

The address is :-

**Planning Reception  
Planning and Development Services Directorate  
Brunel House  
St. Georges Road  
Bristol BS1 5UY.                      Tel: (0117) 922 3097**

The Local Highways Authority may also be able to advise you on general parking issues, highway safety and pavement crossovers. The address is:-

**Director of Highways, Transport and Engineering  
PO Box 87  
Avon House North  
St James Barton  
Bristol BS99 7SG.                      Tel: (0117) 987 1100**

Further information is also available from a number of specialist organisations and local interest groups. Most Conservation or Amenity Societies will be able to advise you on your area. Useful contacts include:

**President  
Bristol Society of Architects  
96, Hotwell Road  
Bristol.                                      Tel: (0117) 929 0413**